



**The following instructions are a reprint from Hotrod Magazine February 1967. The prices and part numbers are no longer valid.**

**Current part number for the side scoops are:**

SSS1 & SSS2 price is \$35.95 per side scoop

**Current part number for our ducting kit is:**

DK1 and price is \$81.95 per kit

*(Prices as of June 2009 and subject to change)*

**PLEASE NOTE:** The ducting kit **cannot** be installed on coupe or convertible cars due to the quarter window being in the way. It will only fit on fastback cars for which it was originally designed.

The side scoops will fit on any 1965-66 Mustang regardless of bodystyle.

You will notice there are no "exact" measurements on the instructions. That is because there are none. Each car was done individually by hand and would therefore be slightly different than any other car. Some common sense and experience doing bodywork on cars is required. (Once the interior is reassembled you will not be able to see the ducting anyway.)

*Branda Performance Inc. is in no way liable for any problems or damages resulting from the modification or attempted modification of your car. If you are in doubt please consult with a body shop that is trained in these types of modifications.*

# WHAT'S THE SCOOP?

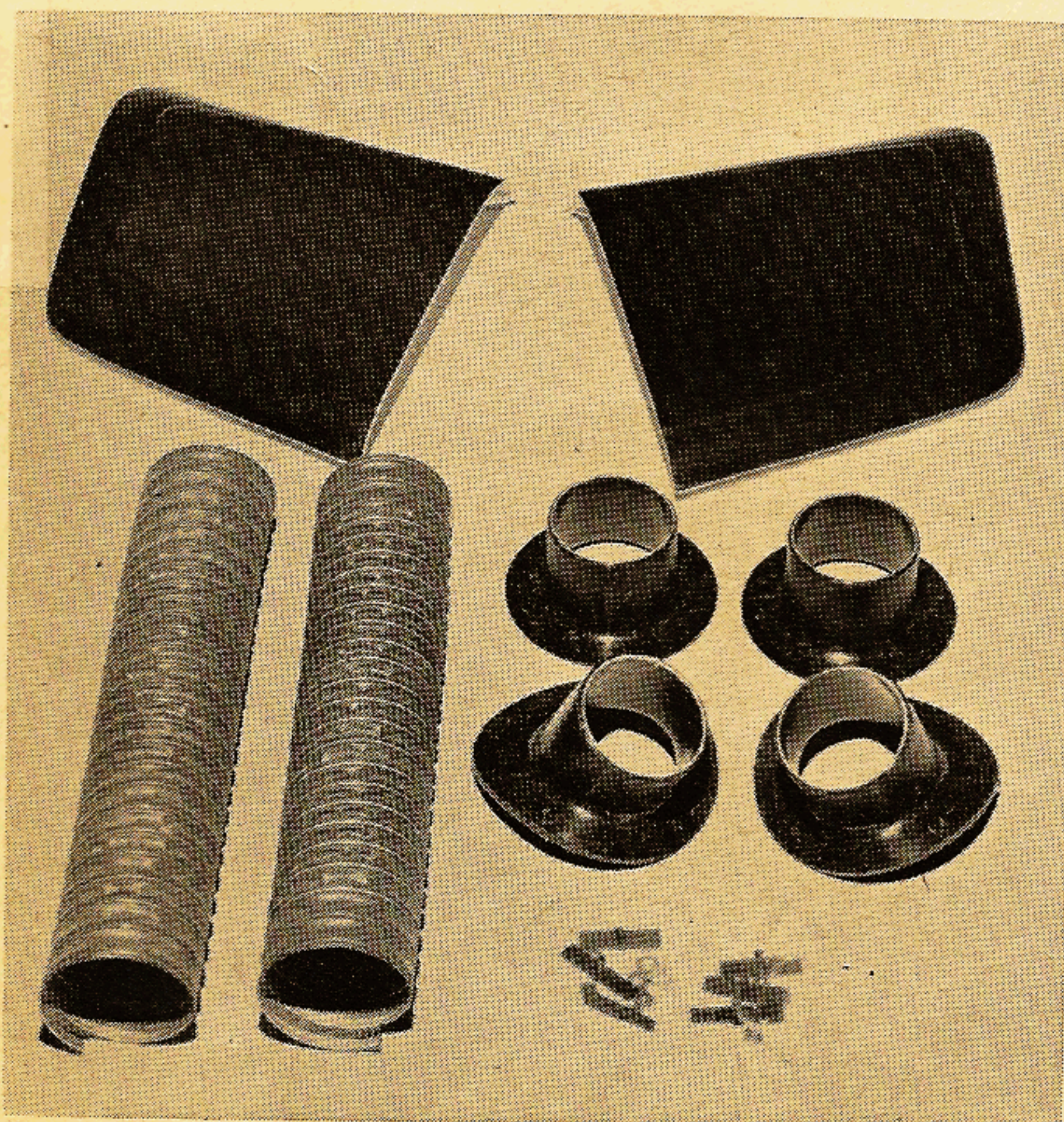
*...a fiberglass kit to aerate Mustang rear binders, that's the scoop*

Text and photos by Bud Lang

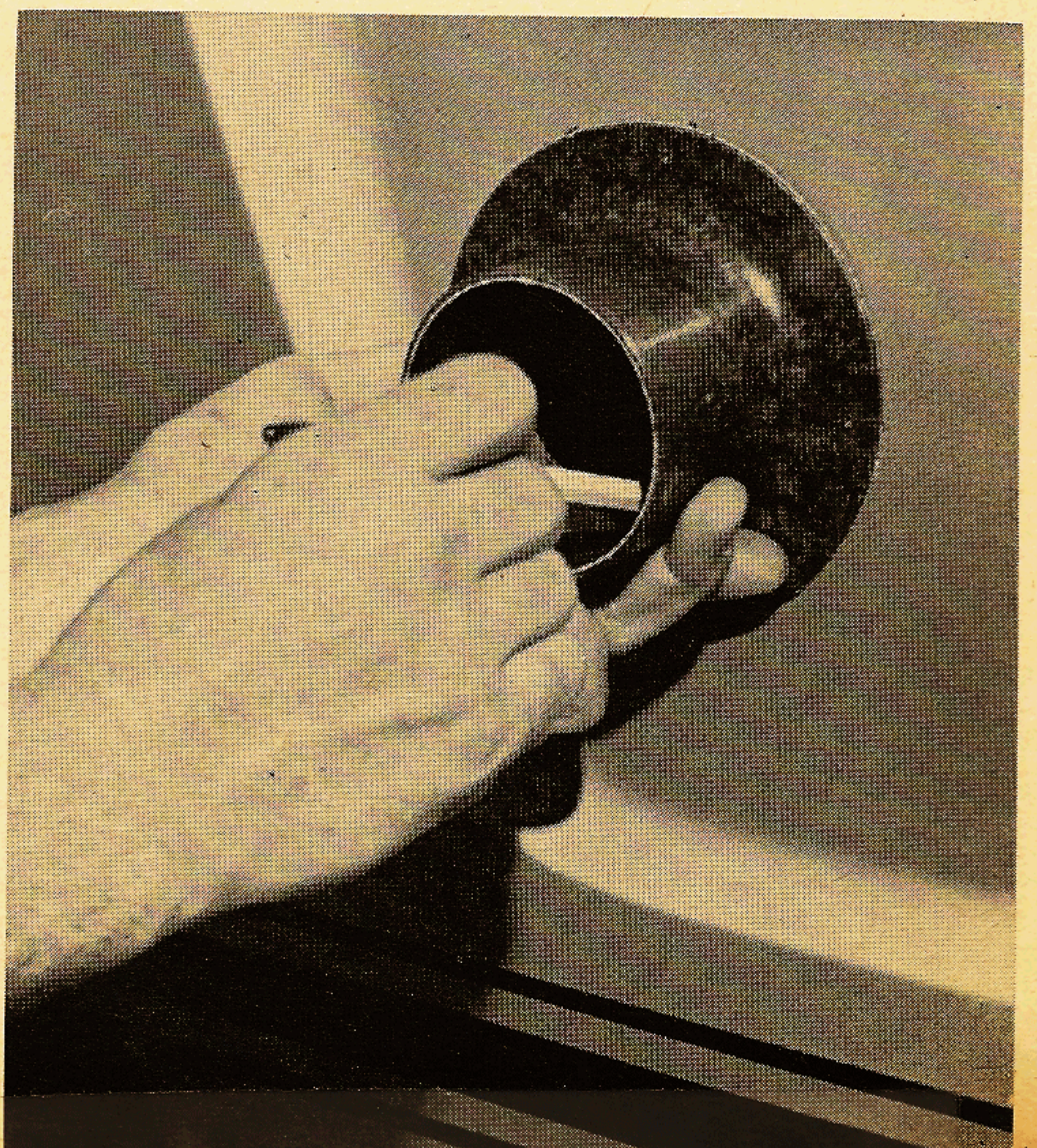
**S**o you own a '65 or '66 Mustang and have wanted a pair of those wild Shelby-American brake scoops, eh, Bunky? Well, fret no longer, because S-A recently released the necessary components in kit form for the do-it-yourselfers. Only a minimum of tools is necessary to complete the installation of these sharp-looking fiberglass scoops and flexible ducting, which feed air back to the rear brake drums. Not shown here, though equally important, are ducts that can be fabricated by the owner and installed either under the car to route air directly to the backing plates, or scoops that will direct air from the flexible duct outlet in the fender well to the backing plates. Listed under part number S2MK 29705-A, the whole package sells for \$29.95. Another point worth mentioning is that you can drill your rear backing plates as illustrated to take full advantage of this air package. Just pull the units from the axle housing before drilling. ■ ■



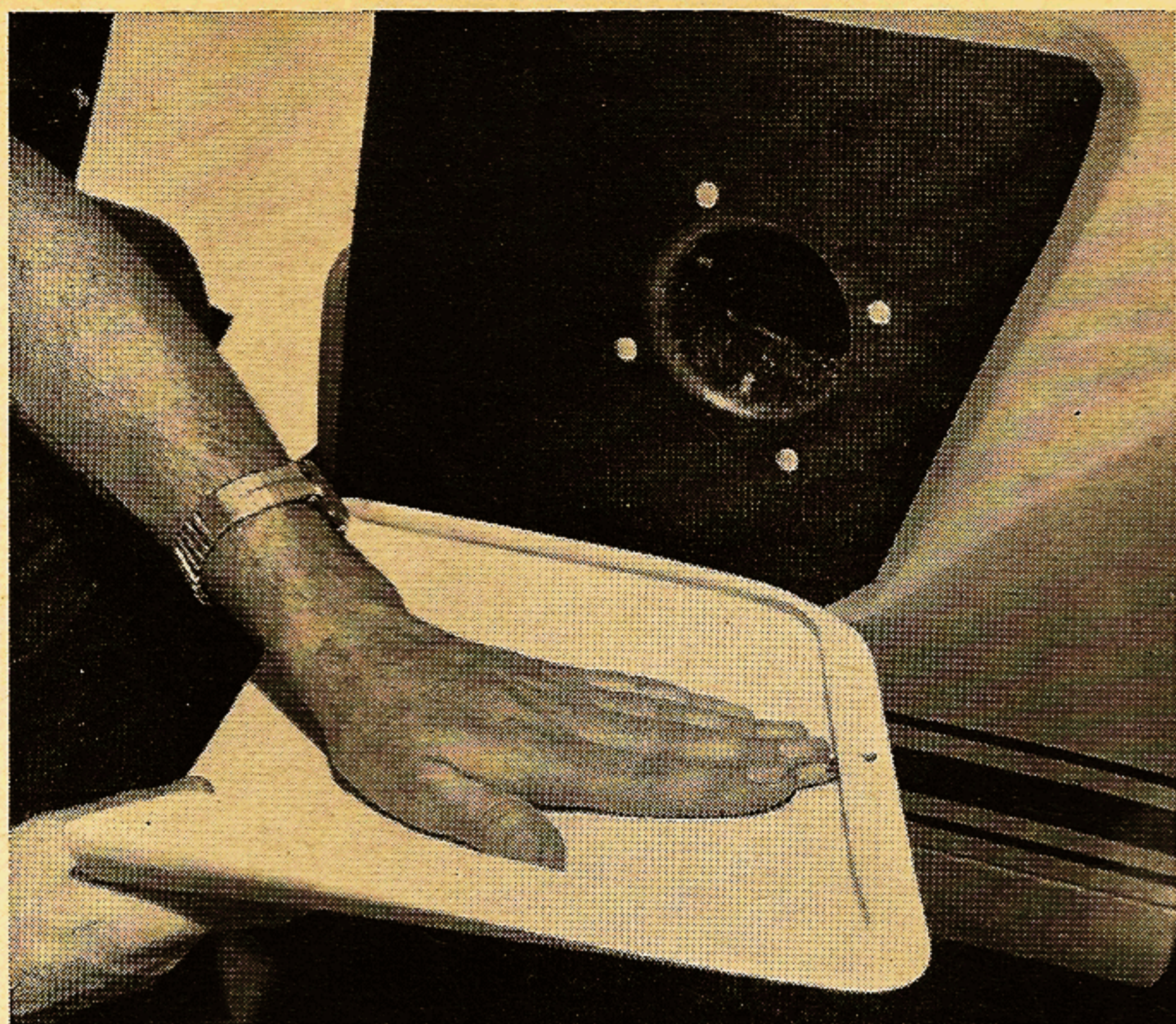
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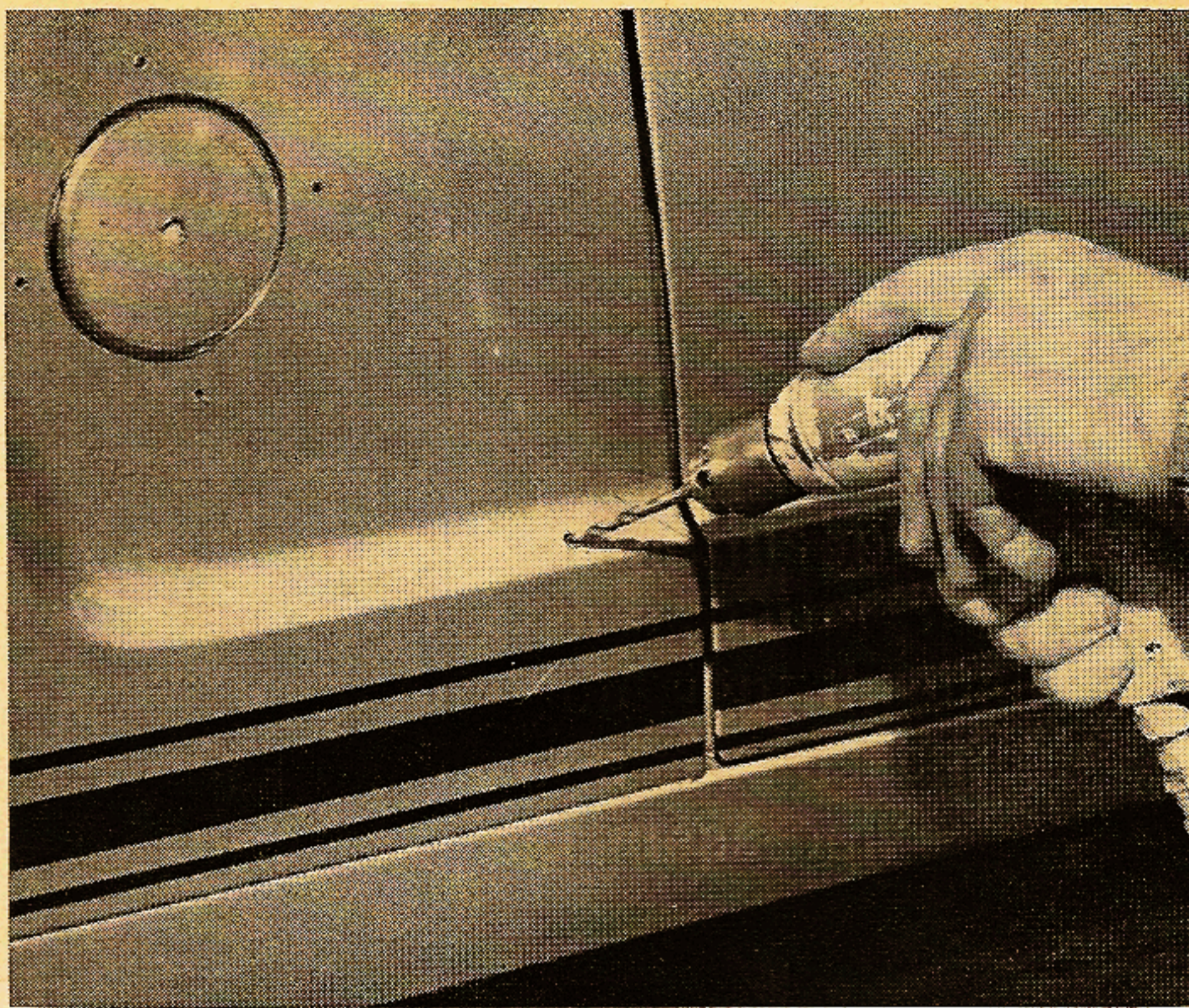
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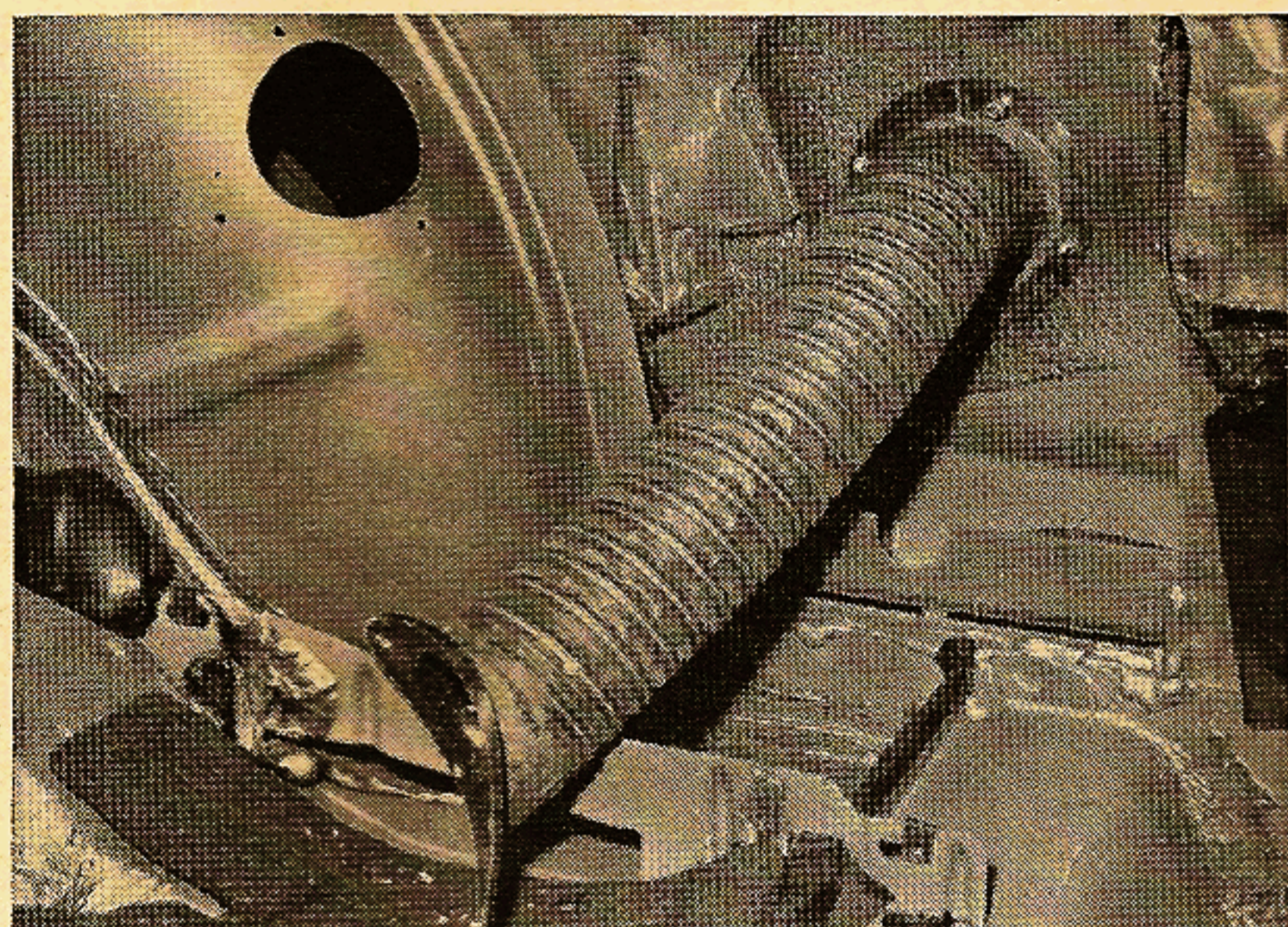
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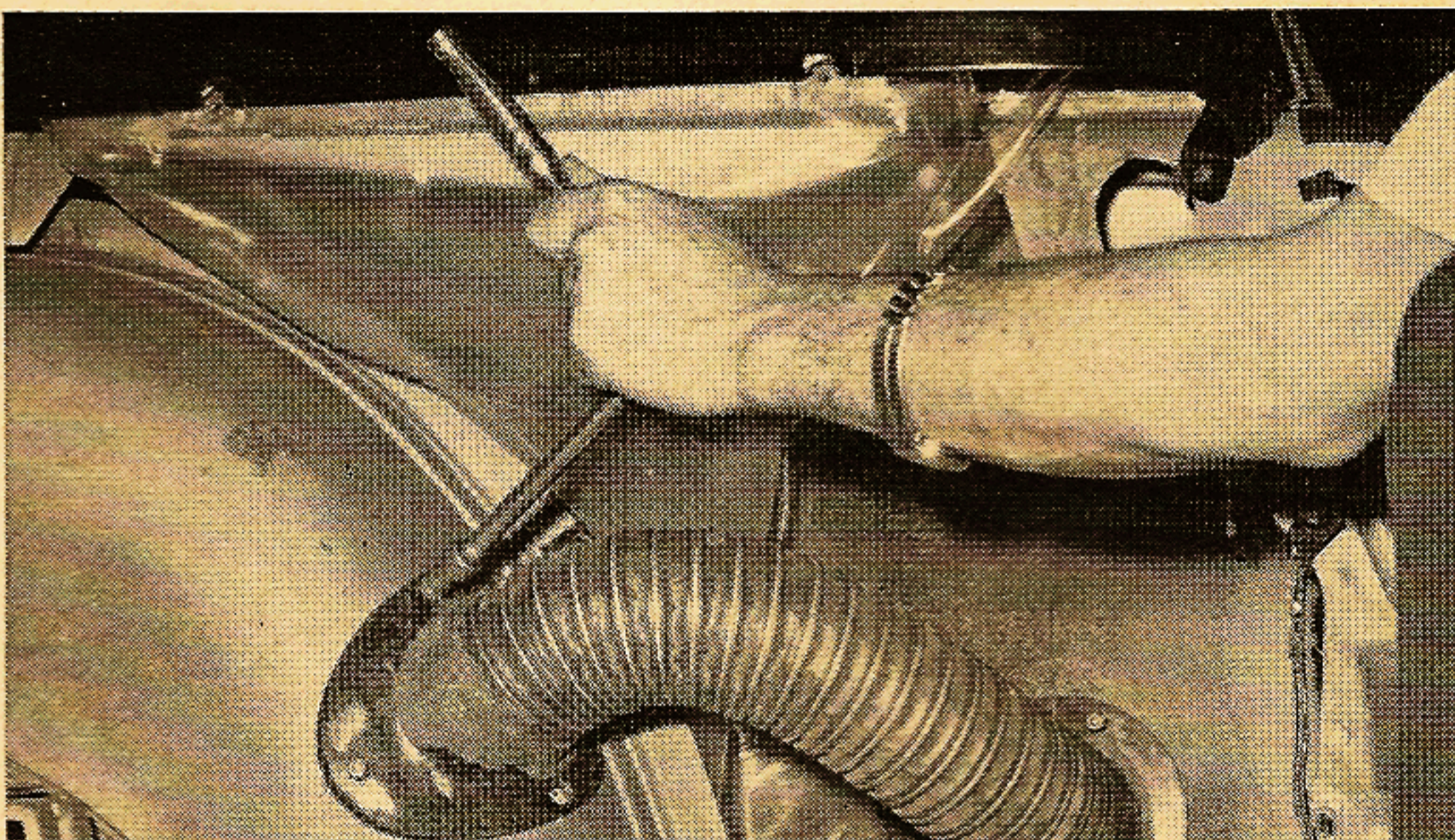
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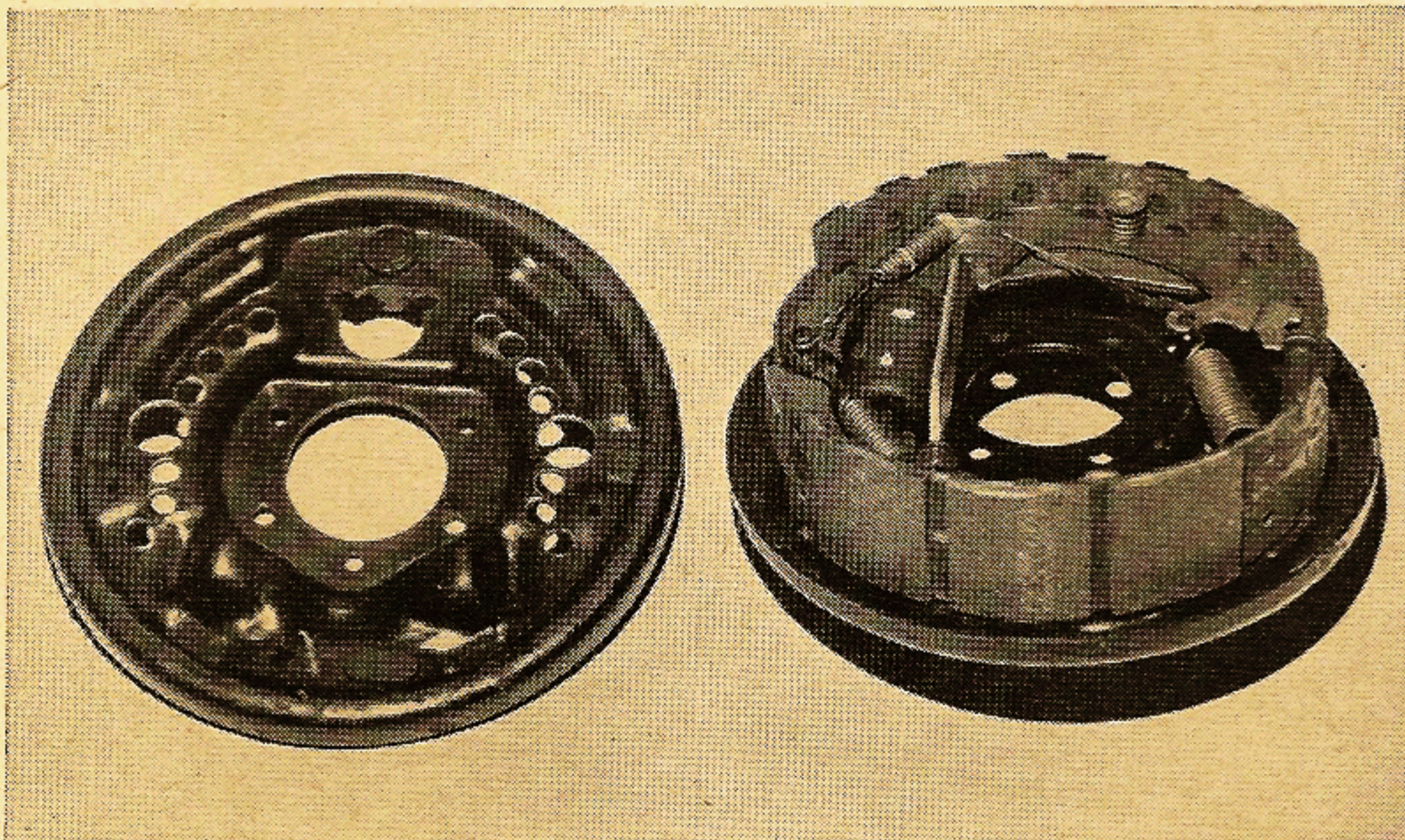
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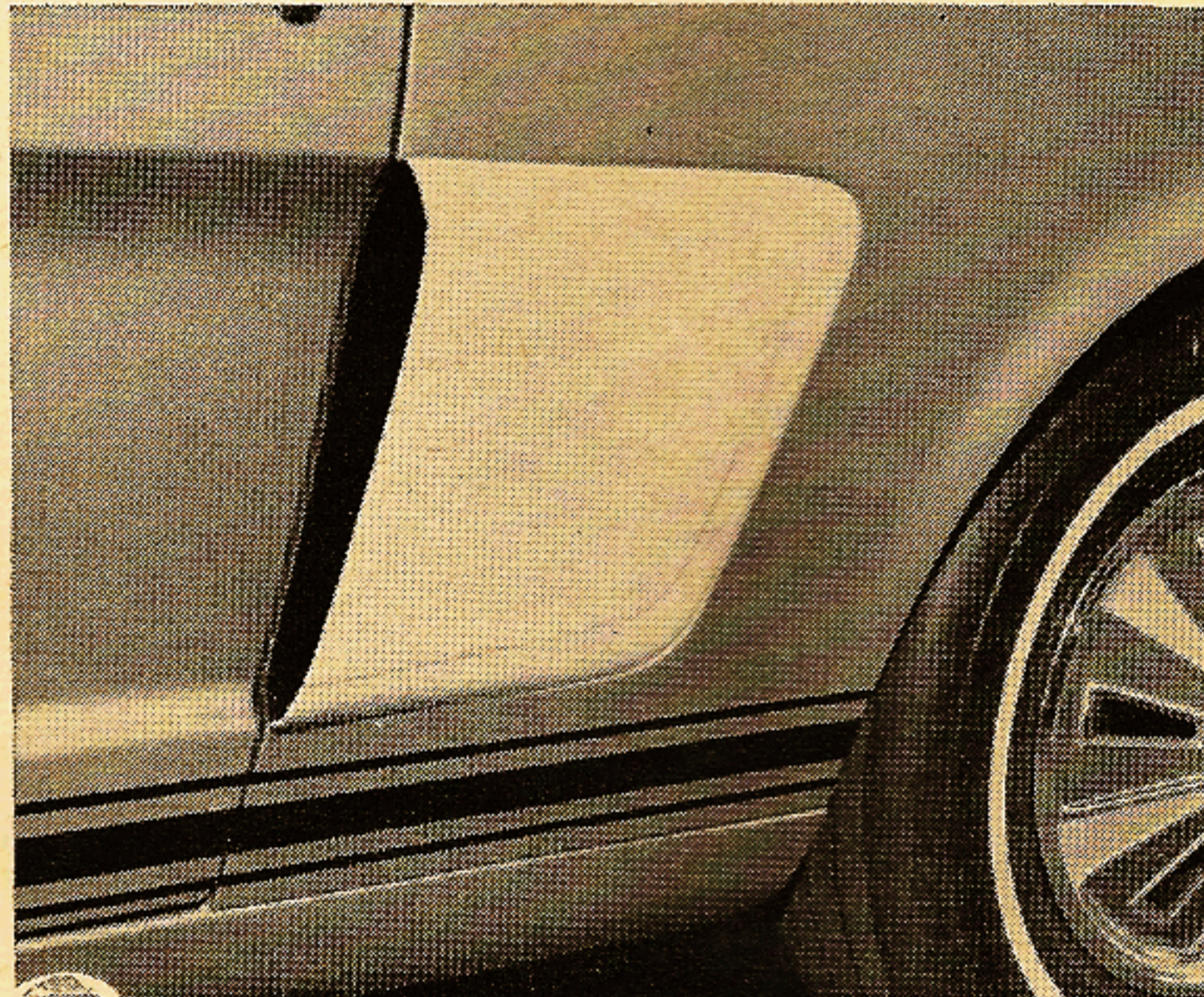
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1. Tim Foraker at Shelby-American demonstrates the ease in which the S-A brake scoop kit for '65-'66 Mustangs may be installed. Body panel that will be under scoop is painted black for effect. 2. Included in the S-A kit are: fiberglass scoops and duct flanges, flexible ducting and necessary bolts and nuts. 3. First step is to center the body flange near the rear end of the natural scallop, mark a circle inside with pencil. 4. A hole saw is used to cut opening in body for duct flange. Four screws hold flange to body. Drill three holes in scoop, one at each edge and one at rear center. By inserting a small pencil in holes,

positioning scoop, moving it slightly, pencil will transfer hole locations to body panel. 5. This view is used to illustrate where duct hole and bolt holes are drilled. 6. At Shelby they coat the fiberglass duct flanges with plastic body filler and use pop rivets, guaranteeing a perfect seal between duct tubing and flanges. You can also, or use metal hose clamps. Drill holes in fender well as shown. It is wise to remove rear seat when attempting this job. 7. You'll need a buddy to hold the bolt from the other side on this one. 8. If you want, drill your backing plates and use metallic linings. 9. Final step is the finish paint.